



BRIZE NORTON NEIGHBOURHOOD PLAN

APPENDIX 26
Version 1 - August 2023

DESIGN GUIDANCE AND CODE

*"our village
our community
our future"*



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INTRODUCTION

Through the Ministry of Housing, Communities and Local Government (MHCLG) Neighbourhood Planning Programme led by Locality, Community First Oxfordshire (CFO) has been commissioned by Brize Norton Parish Council to prepare a Design Code in support of the new Brize Norton Neighbourhood Plan (NP). The National Planning Policy Framework (2021) rooted neighbourhood planning at the heart of the drive for quality development, stating “Design policies should be developed with local communities, so they reflect local aspirations, and are grounded in an understanding and evaluation of each area’s defining characteristics. Neighbourhood plans can play an important role in identifying the special qualities of each area and explaining how this should be reflected in development.” (Paragraph 125).

Neighbourhood Plan Vision: “To support sustainable development that meets the needs of residents now and in the future, while retaining Brize Norton’s separate identity as a rural parish adjacent to, but separate from, the town of Carterton. To protect and where possible enhance our rural environment whilst also acknowledging that RAF Brize Norton will have an ongoing influence over certain elements of our lifestyle.”

This has to be translated in this Design Code taking into account the 10 characteristics set out in the National Model Design Code guidance.

WHY A DESIGN CODE FOR BRIZE NORTON?

This Design Code has been prepared as part of the Brize Norton Neighbourhood Plan (BNNP) to provide guidance on design matters that are specific to the village of Brize Norton and thereby inform any future planning applications especially those for residential development. Local design guides are prepared by local planning authorities and neighbourhood planning groups to set out the general design principles and standards that development proposals should follow in the area, building on policies in the development plan. They are an important way of communicating local design expectations and requirements and are one of the visual tools that the National Planning Policy Framework expects authorities or neighbourhood planning groups to prepare and use.

This Design Code sets out those characteristics that resonate in Brize Norton. It will be proportionate and balanced as a result. Policy is also increasingly concerned with recognising beauty in places and this is reflected in this document. Brize Norton parish comprises the historic settlement with its own character and identity, and beyond this the rural and historic agricultural landscape. In addition to the Neighbourhood Plan policies on character of both the settlement and the landscape the Brize Norton Character Assessment identifies those features within the settlement that contribute to context including heritage, identity, character, and sense of place. It will therefore highlight the characteristics of place that are really important to local people. The Neighbourhood Plan policies provide a hook for more detailed design guidance.

A Design Code is able to address not only design considerations in major developments (i.e., sites of over 0.5 ha or more than ten dwellings), including any District Plan allocations (non-strategic matters include layout, scale, use of materials, and other matters), but also infill developments in the village and the design of individual buildings and spaces, as well as reflecting the aspirations to maintain a living countryside around the village and offset the potential threat from infrastructure pressures across the wider parish.

Also of relevance to the Design Code is the West Oxfordshire Design Guidance which covers all forms of settlement within the district and the Oxfordshire Design Guide which is concerned mainly with streets and creating places that are not car dominated. However, all places are different and have their own qualities and so an overall guide at District level cannot necessarily bring out all the issues of importance to local people, hence the need for a more bespoke Design Guide and Code.

A design code is therefore a tool to:

- add to and support existing policy and guidance;
- clarify what is expected from local developers;
- raise the quality of housing development;
- eliminate inappropriate development; and
- deliver locally distinctive development.

Effective Design Codes are “simple, concise, specific and rely on visual and numerical information rather than detailed policy wording” (National Model Design Code Guidance). The vision for the place needs to be aspirational and set the context for the subsequent development of the code covering matters such as:

- an appreciation of the existing area or site;
- its natural, topographical, historical and heritage features;
- its character and appearance; including patterns of development, density
- the mix of uses and facilities;
- the amount of green infrastructure and character of green space;
- the way in which it deals with traffic, parking, public transport, walking and cycling; and
- sustainability including energy efficiency, net zero alignment and climate resilience.

The National Model Design Guide shows how good design and place making can be achieved by consideration of the 10 characteristics:



Figure 1. National model design guide

APPROACH AND METHODOLOGY FOR DEVELOPMENT OF THE DESIGN CODE FOR BRIZE NORTON

In Brize Norton the process comprised of the following:

Initially work was undertaken by CFO on the Neighbourhood Plan liaising with the NP Steering Group (NPSG) of the Parish Council. The Character Assessment had already been completed by the community including maps and photos and documentary evidence and CFO's work supplemented this with consideration of urban design and localised character analysis based on site walkaround and a review of the evidence base. It is essential consultation including regular communication and liaison with community groups form a key part of the design process from an early stage. The Character Assessment also identifies that the housing in these sub areas might be grouped in three chronological periods and that in each sub area therefore there may be seen various types and properties of varying age.

Walking the village with local residents enabled better understanding of the distinctive qualities of the study area, especially regarding the separate character areas identified. This is important to the Design Guide, supported by a desktop analysis of existing frameworks – landscape and open space, movement, street hierarchy and built form.

With reference to existing adopted Design Guidance in West Oxfordshire and more detailed preparation of design principles appropriate to Brize Norton, and taking into account the policies in the new Neighbourhood Plan, guidelines and codes were developed to be used to inform the design of the parish and future developments. It also means that any developments on a larger scale that might come forward should adhere to the principles and codes laid down in this Design Code. The recent example of Brize Meadow, a large development within the parish, shows what can happen when guidance is not attuned to very local characteristics. The prevailing view is that a more sensitive approach is needed in future so that even if there is a separate free standing area of housing proposed, this Design Code will be relevant and will be followed.

A draft report with design guidance and codes was issued for feedback from the community and finally as part of a larger NP package the Design Code was submitted.

Particular attention was paid to character noting the lanes and streets and pedestrian access, boundaries, building types and spaces, fragmented building lines, varying plot sizes and density, building orientation, land use and mix, heights of structures and materials. In particular, the rural as opposed to urban character of the settlement is noted as an important component worthy of the strongest protection.

The aim of the Design Code is to work from understanding the context and identity to developing the NP vision for each sub area in the parish. The setting for each area is based on the analysis of existing character and Brize Norton's Character Assessment is therefore a key document which provides the context for the Design Code as it relates to each sub area.

This Design Code will therefore set out the vision for the Neighbourhood Plan, with codes that apply for each area type.



CONTEXT AND IDENTITY

The Cotswold Hills are dominated by a thick belt of Oolitic limestone which “plays a defining role in the character of the District. It is the source of the classic, locally dominant building stone, and forms the high, expansive, smoothly rolling plateaux, which ...dip gently towards the south-east.” (West Oxfordshire Design Guide).

The Oolitic limestone of the Cotswolds gives way in successive belts to a capping of crumbly Cornbrash limestone, which forms a distinctive band of rolling landform and finally, to the Oxford clay, river gravels and alluvium of The Upper Thames Vale. Brize Norton occupies part of the band of Cornbrash Limestone which forms the transition between the Oolitic limestone and the clay vale and is one of a string of settlements which traditionally took advantage of the springs that emerge at the junction of the limestone and clay and the lighter more easily worked soils of the Cornbrash. The eastern part of the parish benefits from good arable farming as a result. Brize Norton used these local materials in its development over time as a Cotswold stone village.

Brize Norton is located in the south of the District, in an open and exposed setting. It has a loose-knit originally linear form, following a gravel belt running north-south through the village and with a number of dispersed farms, some of which have been linked by later infill. The settlements in the wolds here are neither visually enclosed nor contained within contours; they range loosely over the landform in a landscape that is open, expansive and gently undulating and vary greatly in elevation, with parts of Brize Norton lying below 80 metres. The farmsteads and farmworkers cottages have now largely been converted to residential uses and supplemented by more recent additions of housing in the 20th century. Much of the traditional services have disappeared ie shops and bakery in the meantime. Despite the dramatic changes the village still feels and looks like a rural village especially as it remains in close proximity to countryside.

RAF Brize Norton covers a large area to the south-west of the village and is a dominant feature in the landscape when seen from Brize Norton and land at a higher elevation to the north..

Linear settlements have a distinctive ribbon form, and have developed historically along both main roads and the smaller side roads that branch off these routes. Development in linear settlements may in parts only be a single house deep on each side thereby allowing significant views into the landscape beyond.

However whilst infilling at Brize Norton has led to a more dispersed pattern of development in parts, the character is still largely linear in form in all of its three main sub areas which are identified in the Neighbourhood Plan and in this Design Code.

Although there has been growth in the latter part of the last century the linear distance of the main part of the village is still walkable. There is a significant degree of connectivity using the PROW network. Despite this, pedestrian access can be restricted within the village itself due to narrow footways, sometimes only on one side of the road and grass verges rather than paved paths in places. Sometimes even in the more spacious developments there will be cars parked on grass verges or pavements which also adds to the issues. Inevitably most journeys out of the village are by car.

The primary roads into Brize Norton comprise Manor Road, Station Road, Carterton Road and Burford Road but all these are unclassified. Monahan Way, which bypasses the village, takes traffic between Carterton and the A40 is a classified B road.

In previous centuries much of this area would have been forested as far south as the clay vale and there was an established route through the Wychwood Forest between Bampton and Shipton under Wychwood which ran through Brize Norton. Today the renaissance of this forest is very much encouraged as part of the Wychwood Project and the prospect of increasing woodland landuse linking the parish of Brize Norton to the Project area and the Cotswold AONB is a desirable objective in the northern part of the parish. This fits with the nature recovery aspirations in the Neighbourhood Plan. There is particular potential to create woodland in the north of the parish as the farmland to the east has a more traditional and fertile agricultural landscape.

The limestone wolds to the north of Brize Norton are characterised by a largely unspoilt rural character as identified in the 1998 West Oxfordshire Landscape Assessment (WOLA) but one that was visually exposed and sensitive to development. It highlighted (in its section on the Shilton Downs which includes Brize Norton) that a particularly strong landscape structure would need to be established to absorb development in more open landscapes.

It also noted that elevated open landscapes are particularly sensitive to tall or prominent structures such as communications masts, large buildings and the newly constructed radar tower in the centre of the village, are relevant concern since the RAF base and its structures have a major visual impact. The WOLA study noted threats to landscape character including agricultural intensification, poor maintenance and loss of field boundaries, expansion of settlements such as around Carterton and suburbanisation of rural settlements and roads. Since 1998 there has been considerable

development in and around Brize Norton but the rolling farmland to the east and north of the settlement has retained its open rural character.

At this stage and as the Neighbourhood Plan makes clear, Brize Norton coalesces with Carterton on its western boundary due to the new development of Brize Meadow which consists of 799 homes. This has effectively urbanised a part of Brize Norton parish. The older sub area housing remains separated by a narrow parkland corridor but faces directly at the building line of these new homes which look very different. It is generally felt that this encroachment of urban development into a rural parish has not been handled sensitively even though design tried to reflect some aspects of the existing village. The new homes appear incongruous with uniformity of design such as regular building lines and roofscapes. There are however no flats in the village.

This Design Code will set out codes to ensure the less acceptable forms of development are not replicated.

THREE SUB-AREAS

The Design Code relies upon the detailed analysis in the Brize Norton Character Assessment. Although this took a chronological approach to character analysis, the Design Code subdivides the settlement into three sub areas each of which was originally grouped around a number of farms and workers cottages. The Character Assessment also identified that the housing in these sub areas might be grouped in four chronological periods and that in each sub area therefore there may be seen various types and properties of varying age. In reality the paramount consideration is that the older pre 20th century housing that underpins village character will dictate design codes. Housing in the period 1927-1971 would not be regarded today as being in keeping with the village character and should be regarded as anomalous in many respects.

On occasions and within visibility of the older areas some character features have been incorporated in developments off side roads and in some places use of materials and fragmented building lines have worked well on more recent schemes. There is concern that new builds tend to have large areas of hardstanding or paved surfacing which again is rather out of keeping with the older areas of the village where large rear/side gardens should be preserved and common open areas kept to a minimum.

The southern section was originally a group of farm houses, farm buildings and workers cottages along Station Road, whilst to the north at the junction of Station Road, Carterton Road and Manor Road was a central area comprising St Britius Church and graveyard, the Manor House, workers cottages, shops and other properties. This central area continues northwards to Chapel Hill with several properties interspersed with open fields and views to wider countryside. In each of these locations there are listed buildings albeit no Conservation Area is designated.

Station Road today is a narrow lane that serves not only the bulk of the housing in Brize Norton but also the north/south through traffic emanating from Carterton and the surrounding areas. Its lack of tranquility as a result of noise and vibration means this is an issue the Design Code should consider. New development at Brize Meadow providing 799 new homes do not access the village directly but rather are accessible from Monahan Way. More development and infilling using Station Road and Manor Road would therefore be detrimental to the character of the village. Beyond the sub areas lies the residual areas of the parish with a rolling agricultural landscape. It should be noted that tree cover is due to be substantially increased in Oxfordshire by 2050 largely and parts of this rolling landscape would benefit from woodland growth including linking up the parcels of ancient woodland.

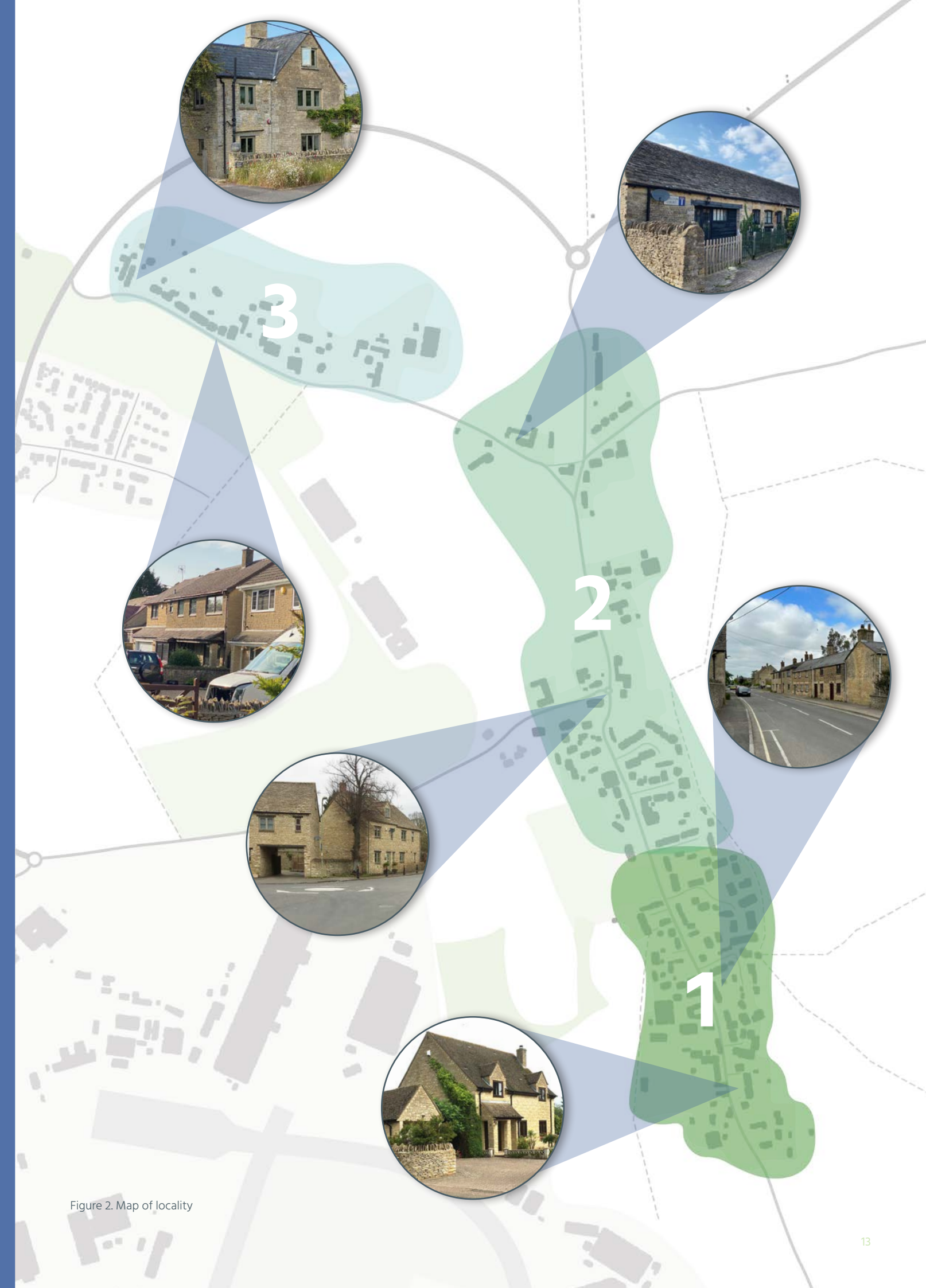


Figure 2. Map of locality

SUB-AREA 1 - THE SOUTHERN SECTION OF BRIZE NORTON

Brize Norton is generally a linear village of stone construction reflecting its origins as a medieval settlement sitting atop limestone and this sub area developed over the centuries around a cluster of farms, farm buildings and farm workers cottages along the lane that is now Station Road. This sub area represents a key area in terms of its stone heritage, the orientation and density of buildings, the materials used for walls and roofs. As with many settlements where use of Cotswold stone is ubiquitous, its use is varied from being cut into smooth ashlar facings for the walls of important buildings and laid as course facings for walls or as coursed rubble for lower status houses, cottages and barns. Roofs are usually Stonesfield slates or Welsh blue slates and this has been carried through into recent infill developments.

There is a strong sense of enclosure to this narrow lane and widths of local side roads are similar to the primary route. Notwithstanding the village is characterised by narrow lanes when compared to urban areas. Station Road only has one footway and cannot be remodelled to cope with greater traffic due to properties and listed buildings being closely sited near the carriageway. New developments should ideally be served by roads other than Station Road.

This sub area broadly runs from the well defined southern gateway to the village to its modern centre where the Elder Bank Hall, sports pavillion, café, recreation ground, allotments, and one of the two remaining pubs (with substantial parking) are located. Most of the older buildings of the settlement are located in this sub area including heritage buildings of which there are 37 Grade II Listed Buildings. Whilst it does not have Conservation Area status the setting of these buildings, often occupying large plots is crucially important to the character and identity of the settlement. For a long time this sub area was separated by green space from the rest of the village to the north and whilst this area has seen some infilling mostly on the eastern side of the road there are still fairly open areas occupied by bungalows, allotments and playing fields near the Elder Bank Hall.

Much of this older part of the village has a low density as a result at around 5 dwellings per acre or 12 per hectare. More recent infill developments such as Honeyham Close have replicated the style of the original Cotswold stone dwellings, constructed from reproduction materials and using a more regular shaped brick to form squared rubble coursed walls. This has been achieved with a higher density (23 dwellings per hectare) and a feeling of spaciousness even though plot sizes are smaller. Some of these buildings are much larger in size however compared to original cottages.



Figure 3. Sub-areas map

Figure 4. Chequered Public House, valued community asset with land to rear



The Neighbourhood Plan suggests that some small areas in this part of the village might have higher densities, such as Chichester Place noted for its use of materials and spaciousness yet achieves a density of 32 dwellings per hectare and this is more typical of modern housing estates. Chestnut Close is even higher density.

Although mostly built off new side roads east of Station Road, development of this type built since the 1970s may now be seen as less appropriate in a village context like Brize Norton compared with past periods. The West Oxfordshire Design Guide notes that whilst a wide range of architectural forms and synthetic materials are available, when used alongside traditional forms and natural materials “may appear alien and out of place”. It goes on to say that the character of historic settlements will be progressively eroded and ultimately lost to future generations and that “good design which responds sensitively to its context, should overcome these problems”.

Unfortunately in Brize Norton development has occurred in the last few decades which adversely impacted the local character and identity and it is now essential that these more recent additions are not replicated. Whilst some effort has been made in the use of materials such as slates for roofing, the height, orientation, building lines, colour palette and even unit sizes have often been less in keeping. With the changes in the National Planning Framework emphasizing beauty in design, it is hoped that any future development within the settlement and its immediate environments will be required to adhere to higher design code thresholds that are in keeping with the village.

The glimpses of open countryside to the east are now more restricted and harder to see from the heart of the settlement. The retention of residual open space to the west of the settlement is even more important as a consequence of the village transforming from a linear to a more dispersed form of settlement. The close relationship of buildings to the primary route (Station Road) through the village provides a sense of enclosure which also generally prevents views through to the RAF base to the west except with a few exceptions. Where new closes have been added over the years, even those to the west of Station Road, they turn their back on

views towards Carterton and the RAF base.

Looking from the south towards the village gives the impression of a framed sort of gateway to the village, the road being enclosed by homes often built close to the road with stone boundary walls and grass verges. Looking west at this point towards the base, it is clearly unsuitable for housing and should remain a green buffer offering some degree of separation as the scale and height of the base is overbearing and dominant and any housing here would be adversely impacted. Whilst development near the road would spoil this gateway into Brize Norton.

However, development on the eastern side of the village offers glimpses across the agricultural landscape which conveys a sense of being a rural village. New closes west of the road provide Cul de sac type development enclosing the street scene and there is a sense of no links westwards other than into fields. To the east of the road, unlike the west side where only small infill is possible, expansion has occurred with side roads serving more houses and with views out into the wider unspoiled countryside including Public Rights of Way links to woodlands towards Curbridge and Lew. It is important to the community that the rural identity of the village is retained and that key views are preserved notably those towards the open countryside east of the settlement.

The historic built form is mostly of stone construction using both oolitic limestone and cornbrash and many former farmworkers cottages are set at 90 degrees to the road traditionally with a footway serving a run of cottages. Unsurprisingly, larger properties and also more recent developments fronting the road have direct access drives onto it, which is not ideal as it is very narrow and busy. There are some areas that might become vulnerable to redevelopment infilling in this sub areas through plot combination but these would need access off a side road rather than directly off the main road. Loss of the one remaining public house, the Chequers, would have a significant impact on all parts of the village and its character. It also offers a through view to open countryside to the east.

Many properties are relatively small and should not be dominated by larger scale of buildings. There is a noticeable absence of extensions and/or flat roof rear additions which would if permitted create a cluttered feel compared to the current situation. Large areas of hardstanding whether brick paving or other material would also engender a more urban perception to grow so grass verges and stone boundary walls many of which are in the “cocks and hens” style of the cotswolds should be preferred.

In some parts of the village even along Station Road itself some bungalow development has been permitted and this creates some variety and a low density

feel in the older established parts of the sub area. Development within visibility of Station Road has been seen as a zone of higher quality so even new houses have to deliver a high standard of design.

SUB AREA 1 - DESIGN CODE



1.1. BUILT FORM

1.1.1. Development layout, building lines, orientation

- Varied building lines should be provided with no regimentation or uniformity.
- Where infill development is proposed access should not be directly be taken from the primary route through the village.
- Access lanes to serve housing should provide sense of enclosure and

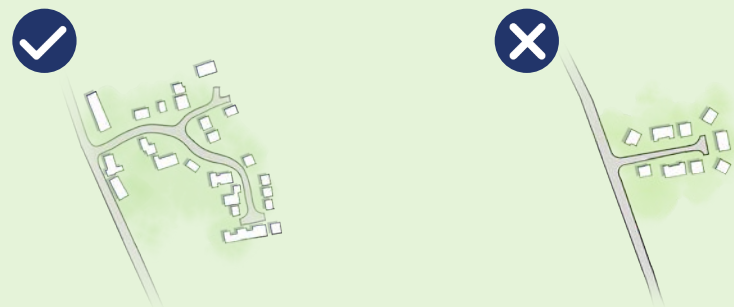


Figure 5. Winding streets provide more spaciousness than straight streets

should avoid being straight more a winding lane especially where visible from Station Road or older pre 20th century parts of the village.

- Spaciousness, using green verges or corners with existing mature trees



Figure 6. Station Road looking north

retained; also to ensure where feasible glimpses to the east through to open countryside.

- Houses to be oriented differently to maximise solar gain where possible and gable end on to Station Road will be favoured.

- Infill schemes should replicate the layout and character of the original village where possible rather than more urban style housing estates.
- Groups of cottages which run at right angles ie. gable end on to the main road should be replicated where possible with vehicular access from a side road



Figure 7. Example of the gable end onto Station Road

and a pedestrian only access from the main road as occurs in several places at present.

- Where opportunities exist to recreate pedestrian access ways to service existing cottages from the road this should be progressed.
- Plots on the west side of Station Road are generally low density with a maximum of circa 12 dwellings per hectare as demonstrated by buildings sited in large plots with occupancy being less than 25% and this should be reflected in any development proposed in that location.
- East of Station Road there will also be limited potential for new development in this sub area but density here could be higher as plot occupancy levels are higher taking post war developments into account. Proposals might be increased in some cases to say 30 dwellings per hectare but very small plots of under 0.5 hectares should not exceed 12 dwellings per hectare as per the west side of Station Road.
- Whilst detached houses front Station Road more of a mix of cottages, bungalows and terraces may be achievable including higher densities on side roads with small front gardens and boundary walls.
- Heritage assets including the setting of listed buildings should receive greater



Figure 8. Properties on Station Road; demonstration of higher densities

protection from plot fragmentation, intensification and other issues arising from new development. The latter should reflect all matters relating to design in regards to this specific context.

- If the opportunity arises to tidy up edges of the builders yard where it abuts the existing settlement edge this could be beneficial to the local community especially if there is scope for rationalizing access for existing homes such as 102-108 Station Road and for increasing garden space for some smaller properties.
- Demolition of any pre 20th century buildings or structures in this sub area will not normally be permitted and where this happens all materials and features should be re-used either in new development or off site in the village.
- Flats or buildings over 2.5 storeys will be unacceptable.

1.1.2. Roofs

Pitched or hipped roofs will be required to achieve some variation in roofscapes in each development to avoid uniformity of design. Flat roof extensions will not be permitted in the village. Village roofscapes should be applied to new development including extensions in the sub areas and the wider parish including:

- Pitched roofs typically 40 – 45 degrees; roofs of slate, reconstituted stone or Cotswold stone this profile and covering.
- Thatched roofs should be avoided as these are more associated with the Clay Vale to the south of the parish (there are only three thatched roofs in Brize Norton).
- In recent times some units have been built larger in size and height (as at South Mere) so care should be taken in design to ensure roofs being over proportioned relative to the front elevation.

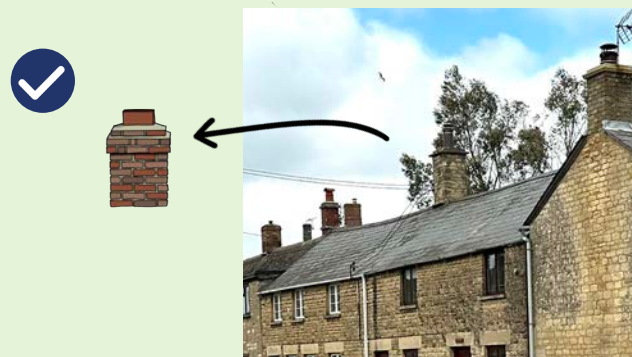


Figure 9. Properties on Station Road; example of the Oxford 'red-brick' chimneys

- Scale including height should reflect sizes of existing properties in the vicinity.
- Red "Oxford" brick chimneys.
- New homes should use similar materials and colour palette to complement existing homes in the older parts of the village i.e. pre 20th century.

1.1.3. Doors and windows

- In keeping with many of the traditional properties, new development should have stone lintels above windows including dormer windows with render.
- Doors may also have porch type structures above them albeit these should not be encouraged on Station Road.
- Doors, windows, chimneys, gates, and boundary walls should all demonstrate continuity with village styles from the pre 20th century.

1.1.4. Boundaries

- Proposed development on Station Road or visible from it (including changes in direction or in association with green verges) should create dry stone wall boundaries, using "cock and hen" method where possible.



Figure 10. Boundaries; traditional cock and hen stone walls favourable

- Boundary timber fences should be discouraged in favour of more open front gardens on side roads. Timber fencing to be avoided on main road frontages and not to be permitted where it interferes with existing access arrangements to older properties.
- Where the footway is insufficiently wide, front gardens should be reduced in size. Limited scope here for grass verges to roadside.



1.2. MOVEMENT

1.2.1. Streets, driveways and parking

- Currently, Station Road is the primary route with secondary road access

off it serving small estates.

- Driveways and garages vary in style and of which there are many links direct onto Station Road, it is important to limit these in future schemes. Garages should be discreetly sited and never in front of the main residence to which they should be subsidiary ideally set back from the building line.
- On-street visitor parking should be provided in laybys.



Figure 11. Electric charging facilities for all new homes

- New housing should only have access drives onto secondary roads not directly onto Station Road, with EV charging for cars and bikes. Parking courts will not be permitted, and all new homes should have driveways or integrated parking in car barns.
- Where possible hard surfacing should be minimal and should be matched by green verges and planted areas. This excludes private garden spaces.
- If Station Road became busier over time it would impact the character of this sub area and its role as a through route might need to be reviewed.
- Light pollution is compounded by the RAF base so street lighting in future schemes should be modest. Screening should also be considered to minimize light pollution from the base.

1.2.2. Cycling

- Bespoke pedestrian and cycling access routes could be enabled to run through open land between Carterton, the RAF base and the settlement to avoid conflict with heavy traffic volumes using Station Road and Carterton Road. These could connect to the outlier areas of the village with the hub near Elder Bank Hall through to the Country Park.



1.3. NATURE AND LANDSCAPE

1.3.1. Protecting views

- Ecologically important areas to the east of the settlement are accessible via Public Rights of Way footpaths and views of open countryside must be

protected as set out in the Neighbourhood Plan especially from within any new development.

1.3.2. Existing natural assets

- Hedgerows should be maintained and enhanced where these form a boundary to a development area or a corridor through the village out to the open countryside, including areas outside the settlement boundary such as along Ting Tang Lane.
- Vintage and Ancient trees must be protected for their landscape value along footpaths but also for their biodiversity value and should be given enhanced status within the parish area. Any felling of any such trees that are unprotected will require mature trees of native species to be replanted upon development being approved.
- Ponds and streams must not be infilled or culverted as a result of new development proposals but rather should be seen as opportunities for



Figure 12. Retained and enhancement of natural landscape features

enhancement as biodiversity corridors and key habitats. New developments should not culvert existing streams but incorporate them as biodiversity assets.



1.4. PUBLIC SPACES

1.4.1. Safeguarding existing local assets

- Public spaces such as the Country Park and other green spaces must be maintained as accessible to all and must continue to function as buffers against coalescence. Stewardship arrangements should be set out under Section 106 Agreements on a long term maintenance basis funded by developers.



1.5. USES

1.5.1. Access

- Opportunities for extra retail provision in the settlement could be

considered where these can be accessible to service traffic without the need to use Station Road and where parking is possible off street.



1.6. HOMES AND BUILDINGS

1.6.1. Sustainability

- New homes should be renewable energy enabled including photovoltaics, heat pumps and such measures to maintain affordability.
- Air source heat pumps should be placed discretely and not visible from the road.



Figure 13. Brize Norton's current allotment provision off Station Road

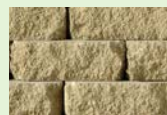
- Food growing areas such as allotments and including some common areas for traditional orchards should be created wherever there is new development planned.

1.6.2. Materials

- Use of traditional materials but also including insulation materials should be implemented in all new developments.
- Where older buildings are included in new development sites every effort should be made to integrate such buildings into the scheme either as homes or outbuildings/garage space and where this is not possible materials should be re-used in the new development.



Cotswold Stone



Reconstituted stone



Oxford red-brick
(chimneys only)

1.6.3. Extensions and conversions

- Changes and additions to existing housing should be in keeping with the existing structure in terms of materials e.g. Cotswold stone if visible from

Station Road. Extensions should be modest in scale, and avoiding flat roofs.

- Barns, and former agricultural buildings should be retained. Any refurbishment should be made using traditional materials.

SUB-AREA 2 - THE NORTHERN SECTION TO CHAPEL HILL

This area from Elder Bank Hall stretches north to the northern gateway to the village at Chapel Hill. Around the church the heritage is especially important and surprisingly there is no designated Conservation Area here. The Design Code needs to pick up the heritage issues here and generally this sub area will have a more protective conservation led attribute. In addition, the countryside is closer here and there are large open gaps separating groups of buildings with open views to the north, west and east.

Many of the characteristics of the southern half of the linear village may also be found here close to the listed church where there is a cluster of older heritage assets often occupying larger plots. The Manor House is listed, as is the Homestead, on Carterton Road. House numbers 2 and 4 on Station Road are also listed. Some of the plot sizes near the church are also large, suiting the large detached houses. Some local refurbishment work is being undertaken in keeping with the village character e.g. The Old Vicarage, west of the roundabout, the Old Bakery has been rebuilt into two cottages with an archway over an access, all of this in traditional style using local



Figure 15. Daubigny Mead; not in keeping with the rest of Brize Norton

materials. The Manor House boundary is demarcated by a high stone boundary wall, very much part of local character. Refurbishment of existing properties is more likely to be the norm in this sub area with few opportunities for infill developments.

Looking west this sub area has a more significant gap provided by fields and green spaces to be retained in future as part of the approved plans for Brize Meadow. Views of Brize Meadow, and of the RAF base convey an urban feel which is accentuated by busy traffic flows along Carterton Road and Manor Road. Whilst Station Road's northern end is included within this sub area it could be regarded as secondary compared to the other two routes. This recent development growth makes the protection of this sub area even more pressing.

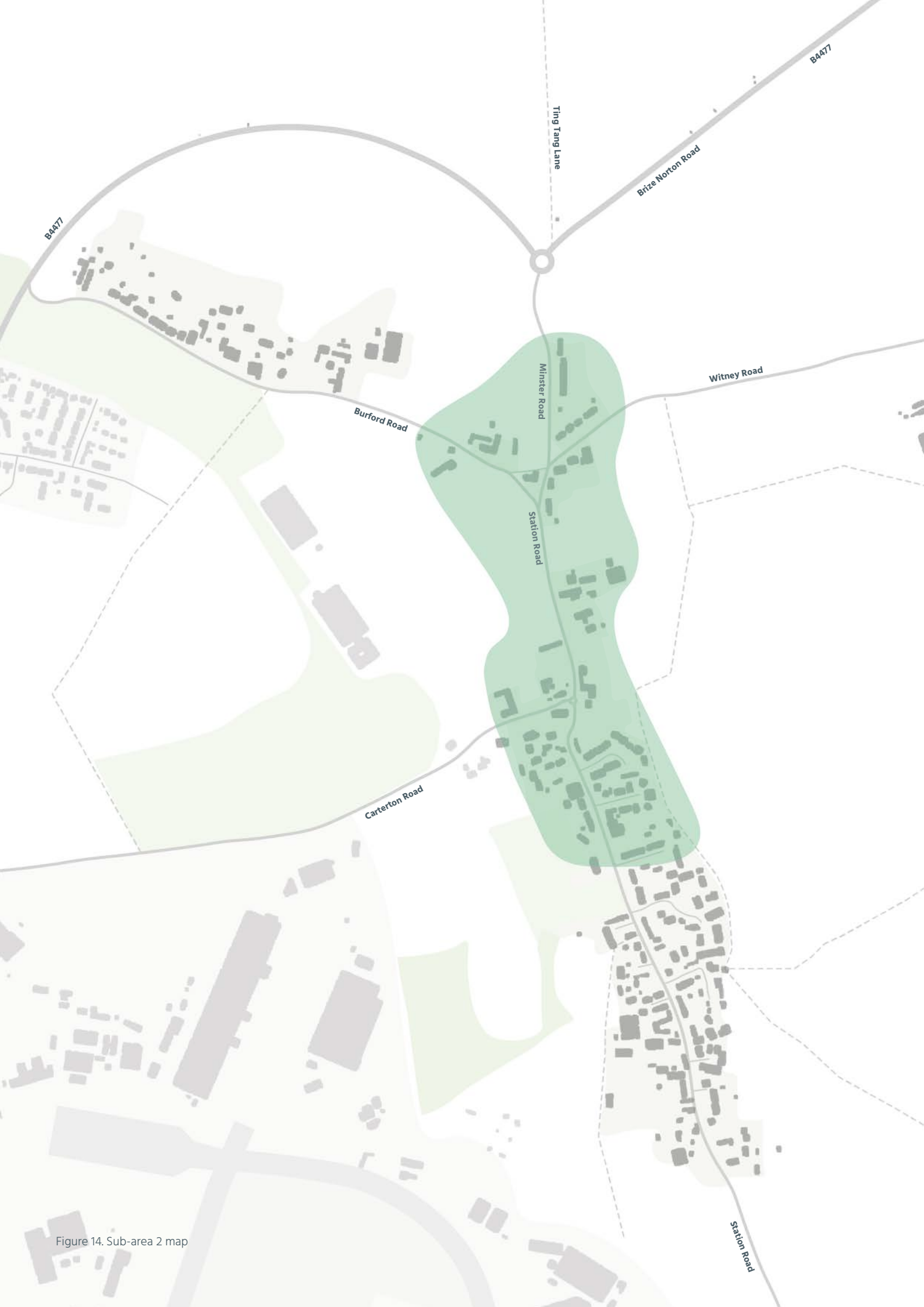


Figure 14. Sub-area 2 map

SUB AREA 2 - DESIGN CODE

Some small infill schemes have slotted in well near the church such as Moat Close which is of similar scale and provides green spaces. Infill development may actually be better able to achieve good design than larger developments such as Brize Meadow which tend to follow the larger housebuilder templates. Whilst opportunities may arise east of Manor Road the settlement boundary may be a major constraint. There are bungalows in some areas such as Squires Close and at the Fosseway. At Daubigny Mead, regimented lines of housing look out of place with uniform roofscapes all of similar heights.

Moving northwards out of the village, the plots are generally larger and there is a feeling of greater spaciousness and views over open countryside in all directions. At Chapel Hill there is a definitive end to the character sub area with a cluster of old buildings near this northern entry point into the village. There is only one barn left unconverted in this sub area. Its inclusion in any future development should be protected either under refurbishment or being integrated. A successful example of that exists near the Old Manor House.

There are fewer opportunities for new side roads and infill development in this sub area. Off Station Road there may be scope but the constraint along the eastern boundary of the settlement will impose significant limitations. Notwithstanding there are more recent developments such as Daubigny Mead, these are out of keeping with the historic character of the village and should not be replicated. North of Elm Grove, brick/render "Wheatley Act" council houses built from the late 1920s onwards are present, being mostly semi-detached with decent sized gardens.

Some of these outliers front Minster Road and whilst forming part of the northern gateway to the settlement, they are not part of the stone-built village that starts really at Chapel Hill and do not add much in terms of character. Beyond these is open countryside, mostly on higher ground with lengthy views over the area towards Carterton and the RAF base.



2.1. BUILT FORM

2.1.1. Development layout, building lines, orientation

- Varied building lines should be provided with no regimentation or uniformity.
- Access lanes to serve housing should provide sense of enclosure and should avoid being straight in favour of a more winding lane.
- Spaciousness, using green verges or corners with existing mature trees retained within schemes and including here on Manor Road ; also to ensure where feasible glimpses to the east through to open countryside
- Houses to be oriented to maximise solar gain where possible and gable



Figure 16. Gable end on to Station Road

end on to Manor Road/Station Road will be favoured.

- Where infill development is proposed access drives or garages should ideally not be directly approached from the primary routes through the village.
- Infill schemes should replicate the layout and character of the original village where possible rather than more urban style housing estates.
- Groups of cottages which run at right angles i.e. gable end on to the main road should be replicated where possible with vehicular access from a side road and a pedestrian only access from the main road as occurs in several places at present.
- Where opportunities exist to recreate pedestrian access ways to service existing cottages from the road this should be progressed
- Developed plots should be lower density with a maximum of circa 12

dwelling per hectare in this sub areas to be in keeping with older parts of the village and where density is higher such as at Daubigny Mead (est 27 dwellings per hectare) these should be considered excessive, outdated and anomalous.

- Buildings sited in large plots with occupancy being less than 25% should be reflected in any development proposed in this sub area.
- Spaciousness should be prioritized in all new schemes including non residential development.
- Whilst detached houses front Manor Road /Station Road more of a mix of cottages, bungalows and terraces may be achievable including higher densities on side roads with small front gardens and boundary walls.
- High stone boundary walls are more of a feature around prominent properties and must be retained – eg if there is proposed backland development these must not be compromised.
- Heritage assets including the setting of listed buildings should receive greater protection from plot fragmentation, intensification and other issues arising from new development. The latter must reflect all matters relating to design in regards to this specific context.
- There is no Conservation Area but future designation should not be compromised by more contemporary designs. Use of traditional materials including stone and slates, colour palettes, roofscapes, traditional windows and doors should all be applied to the highest possible standard to proposals within view from the church or the Old Manor House otherwise within say 400m of it.
- The new building known as the Old Bakery has successfully met these



Figure 17. 'Old Bakery' restoration

requirements. On more prominent buildings or those that might stand out such as in a corner position higher quality ashlar finishes might be applied.

- Demolition of any pre 20th century buildings or structures in this sub area will not normally be permitted and where this happens all materials and features should be re-used either in new development or off site in the

village.

- Flats or buildings over 2.5 storeys will be unacceptable.



2.2. MOVEMENT

2.2.1. Streets, driveways and parking

- New housing should only have access drives onto secondary roads not directly onto Manor Road / Station Road, with EV charging for cars and bikes.
- Parking courts will not be permitted, and all new homes should have driveways or integrated parking in car barns.
- On-street visitor parking should be provided in lay-bys on side roads. Where possible hard surfacing should be minimal and should be matched by green verges and planted areas, excluding private garden spaces.
- The approach into Brize Norton from the east via Elm Grove is very open and elevated and offers views of the large structures at the RAF base and additional structural planting would improve the outlook in this area.
- Where new side roads might be created these should be more like streets with no wide bell mouthed junctions
- Light pollution is compounded by the base so street lighting in future schemes should be modest. Screening should also be considered to minimize



Figure 18. Parking examples

light pollution from the RAF base.

- Bespoke pedestrian and cycling access routes could be enabled to run through open land between the RAF base and the settlement and to avoid conflict with heavy traffic using Manor Road/Station Road.
- There is limited scope for a new road to connect land to the east of the village with the existing road network in this sub area. Any such proposals would be unacceptable on grounds of damage to landscape character, visual intrusion, impact on identity as loss or rural character.



2.3. NATURE AND LANDSCAPE

2.3.1. Protecting views

- Ecologically important areas to the east of the settlement are accessible via Public Rights of Way footpaths and views of open countryside must be protected as set out in the Neighbourhood Plan especially from within any new development.

2.3.2. Existing natural assets

- Hedgerows should be maintained and enhanced where these form a boundary to a development area or a corridor through the village out to the open countryside, including areas outside the settlement boundary such as along Ting Tang Lane.
- Vintage and Ancient trees must be protected for their landscape value along footpaths but also for their biodiversity value and should be given enhanced status within the parish area. Any felling of any such trees that are unprotected will require mature trees of native species to be replanted



Figure 19. Ting Tang Lane

upon development being approved.

- Ponds and streams must not be infilled or culverted as a result of new development proposals but rather should be seen as opportunities for enhancement as biodiversity corridors and key habitats.
- Larger development areas that require a Masterplan should incorporate long term stewardship via S106 or other suitable mechanisms, to ensure large areas of green space are retained of not less than 50% of the gross area providing for play areas, public spaces, playing pitches. At least half of the greenspace should be set aside for nature recovery and a LEMP will be required to achieve these outcomes.



2.4. PUBLIC SPACES

2.4.1. Safeguarding existing local assets

- Public spaces such as the Country Park and other green spaces must be maintained as accessible to all and must continue to function as buffers against coalescence.



2.5. USES

2.5.1. Access

- Opportunities for non residential development will be limited to conversions but could be considered provided there is off street parking provision or a lay by for short visits, also that these do not congest the main road through the village.



2.6. HOMES AND BUILDINGS

2.6.1. Sustainability

- New homes should be renewable energy enabled including photovoltaics, heat pumps and such measures to maintain affordability. As there are heritage issues these technologies would need to be deployed discreetly but it is possible that listed buildings can incorporate them into their settings.
- Food growing areas such as allotments and including some common areas for traditional orchards must be created wherever there is new development planned.



Figure 20. Brize Norton's Allotment

2.6.2. Materials

- Use of traditional materials but also including insulation materials should be implemented in all new developments.

Figure 21. Two detached houses on Carterton Road



- Where older buildings are included in new development sites every effort should be made to integrate such buildings into the scheme either as homes or outbuildings/garage space and where this is not possible materials should be re-used in the new development.

2.6.3. Extensions and conversions

- Changes and additions to existing housing should be in keeping with the existing structure in terms of materials e.g. Cotswold stone especially if visible from Station Road. Extensions should be modest in scale, and avoiding flat roofs.



Cotswold Stone



Reconstituted stone

Oxford red-brick
(chimneys only)

- Barns, and former agricultural buildings should be retained. Any refurbishment should be made using traditional materials.

SUB AREA 3 - BURFORD ROAD OUTLIER

This location along Burford Road grew as a hamlet and there is a long run of housing north of the lane all the way from Kings Barn to the western end of the line of housing. Garston Court forms part of sub area 2 and there is significant open land between that and the start of this outlier at Kings Barn. In these terms Burford Road is the primary route through this sub area although it remains a quiet country lane with no direct connection to Brize Meadow which within view but is several hundred metres distant.

Building lines are fairly consistent offering green spaces in front of houses and more recent infill housing has slotted into the existing pattern of development.

This formerly tranquil and visually attractive rural scene has been spoiled somewhat by the advance of higher density housing from Brize Meadow. As a result the views enjoyed previously over open countryside no longer exist and though a modest area of green space separates the sub area from the new development, the scale of the latter is dominant. Unfortunately this approach seems to have followed a pattern of housing bordering onto green space as has happened nearer to Shilton parish boundary where the new Kilkenny Park provides a buffer and this green theme has continued around the North and North east side of Carterton with Brize Norton parish.

Most of the housing on Burford Road is of stone construction and to the east and north there are still substantial green areas of countryside including a small collection of older buildings former farmsteads near the junction of Kilkenny Lane and Monahan Way. Many properties are detached along Burford Road with open areas of lawn or large front curtilages to these plots which are often larger than in other parts of the village. Substantial rear gardens are also prevalent.

Currently densities and plot occupancy (sub 20%) along Burford Road are the lowest of the three sub areas and this needs to be reflected in this Design Code. Much is below 10 dwellings per hectare.

This sub area exhibits much of the old character of Brize Norton with traditional materials and styles, colour palettes such as Cotswold stone, sizes of plots and houses, roofscapes, stone elevations, windows with lintels and door types all on display. There are some particularly fine and prominent houses set back from the road with ashlar and dressed stone finishes. Slate roofing is very much evident. There are former farmhouses, farm buildings and large houses of far greater scale than in the



Figure 22. Sub-Area 3 map

SUB AREA 3 - DESIGN CODE

rest of Brize Norton suggesting its location as formerly one of the more affluent areas.

This locale would have been quite remote until recently and is not easily visible from adjoining areas as it occupies lower ground compared to Brize Meadow.

Any new infill development here would require careful assessment and attempts to integrate new housing into backland potentially linking up with areas close to Monahan Way would demand the highest quality designs and low density layouts.

This is the most rural of the three sub areas with views over countryside to the north, east and south but since Brize Meadow has been built new infill development might prefer to be reorientated to face away from it or set back from Burford Road so there is no scope for overlooking.



3.1. BUILT FORM

3.1.1. Development layout, building lines, orientation

- Varied building lines should be provided with no regimentation or uniformity
- Spaciousness, using green verges and open spaces in front of houses on main roads
- Infill schemes should replicate the layout and character of the original village where possible rather than more urban style housing estates like Brize Meadow.
- Buildings that gable end on to the main Burford Road should be replicated where possible with vehicular access from a side lane and a pedestrian only access from the main road as occurs in several places at present.
- Where opportunities exist to recreate pedestrian access ways to service existing cottages from the road this should be progressed
- Developed plots should be lower density with a maximum of circa 10 dwellings per hectare in this sub areas to be in keeping with older parts of the village and where larger schemes are proposed in this sub areas the density levels should not be exceeded.
- Buildings sited in large plots with occupancy being less than 20% should be reflected in any development proposed in this sub area.
- Spaciousness should be prioritized in all new schemes.
- High stone boundary walls are more of a feature around prominent properties and should be retained – eg if there is proposed backland



Figure 23. High traditional stone wall boundaries

development these must not be compromised.

- Heritage assets including the setting of listed buildings should receive greater protection from plot fragmentation, intensification and other issues arising from new development. The latter must reflect all matters relating to design in regards to this specific context.
- There is no Conservation Area but future designation should not be compromised by more contemporary designs. Use of traditional materials including stone and slates, colour palettes, roofscapes, traditional windows and doors should all be applied to achieve the best design outcome.
- Demolition of any pre 20th century buildings or structures in this sub area will not normally be permitted and where this happens all materials and features should be re-used either in new development or off site in the village.
- Flats or buildings over 2.5 storeys will be unacceptable.

3.1.2. Roofs

- (pitched roofs typically 40 – 45 degrees) roofs of slate, reconstituted stone or Cotswold stone this profile and covering.
- Thatched roofs should be avoided as these are more associated with the Clay Vale to the south of the parish (there are only three thatched roofs in Brize Norton)
- In recent times some units have been built larger in size and height so care should be taken in design to ensure roofs being over proportioned relative to the front elevation and to adjacent older properties.
- Scale including height should reflect sizes of existing properties in the vicinity.



Figure 24. Example of the scale and style of the dwellings in sub-area 3



- Uniformity of roof ridge lines will not be acceptable and houses will need to be oriented differently
- Red “Oxford” brick chimneys should be provided.

- New homes should use similar materials and colour palette to complement existing homes in the older parts of the village i.e. pre 20th century.

3.1.3. Doors and windows

- In keeping with many of the traditional properties, new development should have stone lintels above windows including dormer windows with render.
- Doors may also have lintels and even porch type structures above them.
- Doors, windows, chimneys, gates, and boundary walls should all demonstrate continuity with village styles from the pre 20th century.

3.1.4. Boundaries

- Proposed development on Burford Road or visible from it should provide green verges where possible and high dry stone wall boundaries, using “cock and hen” method where possible; large setbacks from the road and long rear gardens should be retained and replicated in new development where possible.
- Boundary timber fences should be discouraged in favour of front gardens on to Burford Road.



3.2. MOVEMENT

3.2.1. Streets, driveways and parking

- This is an area of old established properties on Burford Road, once an important link but at this juncture a bit of a backwater, a tranquil rural lane offering views over the new Mary Ellis Country Park. Monahan Way provides a clear route to Carterton avoiding Burford Road. Traffic should not be encouraged to use Burford Road other than for access to properties.
- Large front gardens and drives can enable EV charging for cars and bikes.



Figure 25. EV Charging for any new developments

Parking courts will not be permitted, and all new homes will have driveways or integrated parking in car barns or at the rear of properties.

- On-street visitor parking should be provided in laybys in side roads. Where possible hard surfacing should be minimal and should be matched by green verges and planted areas which exclude private garden spaces.
- The area abuts the northern gateway to the village where different routes converge and there may be scope for some limited infilling here.
- New housing should not be connected to the very narrow Kilkenny Lane.
- Bespoke pedestrian and cycling access routes could be enabled to run through open land between Brize Meadow and the sub area and to avoid



Figure 26. New development of Brize Meadow in Brize Norton

conflict with heavy traffic using Manor Road, Station Road, and Carterton Road. These cycle routes should utilize the Country Park.



3.3. NATURE AND LANDSCAPE

3.3.1. Protecting views

- Ecologically important areas to the north of the settlement are accessible via non vehicular routes such as Ting Tang lane and views of open countryside must be protected as set out in the Neighbourhood Plan especially from within any new development.

3.3.2. Existing natural assets

- Hedgerows should be maintained and enhanced where these form a boundary to a development area or a corridor through the village out to the open countryside, including areas outside the settlement boundary.
- Vintage and Ancient trees must be protected for their landscape value along footpaths but also for their biodiversity value and should be given enhanced status within the parish area. Any felling of any such trees that are unprotected will require mature trees of native species to be replanted upon development being approved.
- Ponds and streams must not be infilled or culverted as a result of new development proposals but rather should be seen as opportunities for enhancement as biodiversity corridors and key habitats.



3.4. PUBLIC SPACES

3.4.1. Safeguarding existing local assets

- Public spaces such as the Country Park and other green spaces must be maintained as accessible to all and must continue to function as buffers against coalescence.



3.5. HOMES AND BUILDINGS

3.5.1. Sustainability

- New homes should be renewable energy enabled including photovoltaics, heat pumps and such measures to maintain affordability. As there are heritage issues these technologies would need to be deployed discreetly but it is possible that listed buildings can incorporate them into their settings.
- Food growing areas such as allotments and including some common areas for traditional orchards must be created wherever there is new development planned.

3.5.2. Materials

- Use of traditional materials but also including insulation materials should be implemented in all new developments.
- Where older buildings are included in new development sites every effort should be made to integrate such buildings into the scheme either as homes or outbuildings/garage space and where this is not possible materials should be re-used in the new development.



Cotswold Stone



Reconstituted stone

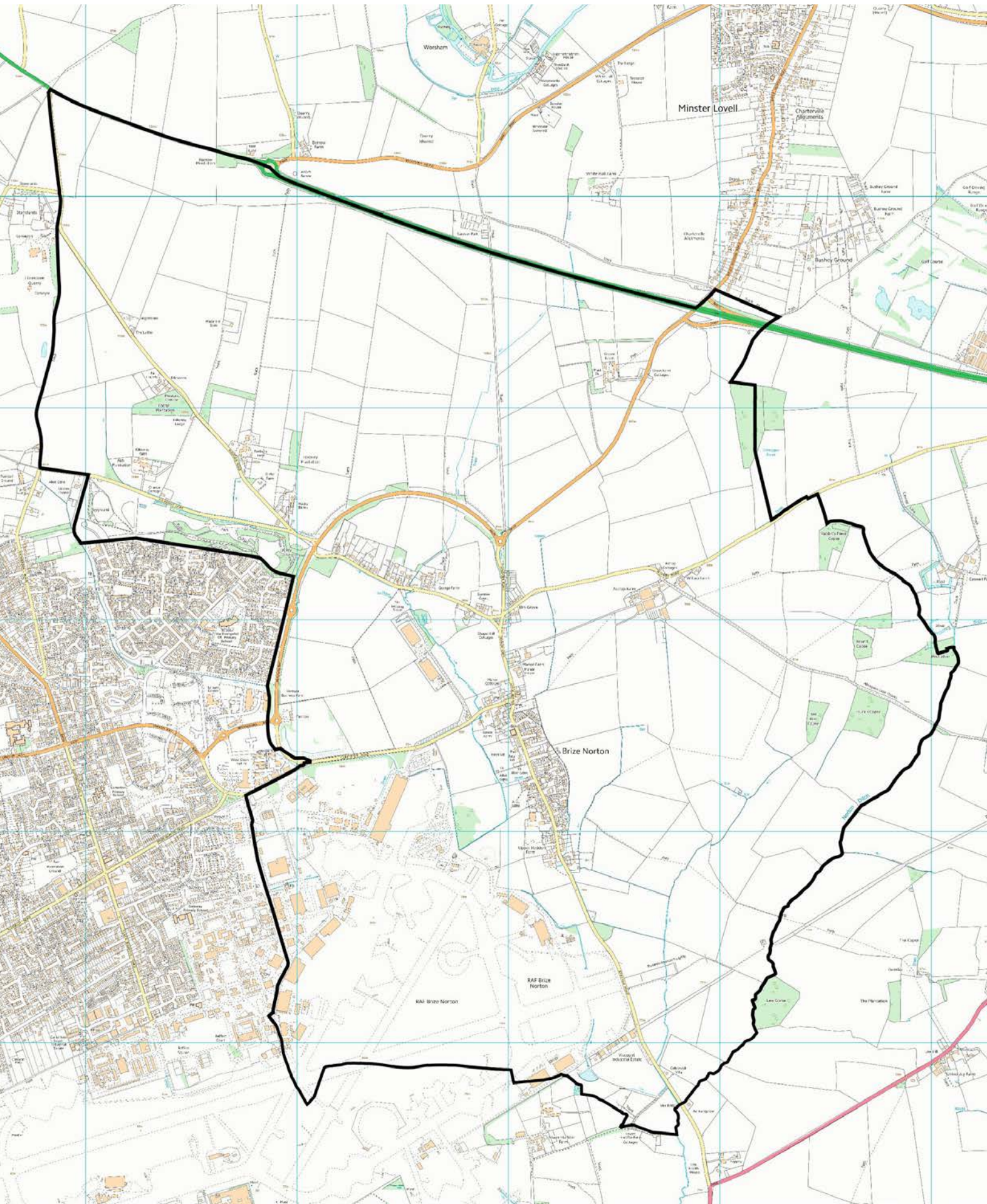


Oxford red-brick only for chimney

3.5.3. Extensions and conversions

- Changes and additions to existing housing should be in keeping with the existing structure in terms of materials e.g. Cotswold stone if visible from Burford Road. Extensions should be modest in scale, and avoiding flat roofs.
- Barns, and former agricultural buildings should be retained. Any refurbishment should be made using traditional materials.

Figure 27. Parish boundary of Brize Norton



RESIDUAL AREAS OF THE PARISH

The rural areas of the parish stretch out to the north and east of the settlement comprising rolling fields with hedgerows and pockets of woodland.

It is expected and desired that these parts of the parish will retain their character and identity and will in addition see biodiversity enhancement. It is already accepted that Nature Recovery plans will be implemented here and that furthermore linking up with the Wychwood Forest Project and the AONB north of the A40 will allow walking and cycling from Brize Norton in this direction. Extensions to the existing Country Park to enhance its setting will be important assets for the growing community.

To the east the views from the village over open agricultural countryside will also be retained although it may be that in time new Country Parks are created to the east towards Curbridge. The existing road network in that location will not be improved other than to provide for safer links for off road walking and cycling routes through to the village. In this area individual buildings or groups of existing buildings should be retained and converted where possible where these are life expired.

As the Neighbourhood Plan aims to prevent further coalescence with settlements to the west, sprawl to the east and north will also be resisted. Extending and linking up pockets of Ancient Woodland will be welcomed.

To the west of the settlement the remaining precious areas of green space should be retained and protected from development including the gap between the RAF base and the settlement. Here there may be scope for some improved links to enable new development to avoid being connected to Station Road with a link to Carterton Road that avoids using this narrow artery. Whilst the retention



Figure 28. South entrance to the village

of the southern gateway to the village is very important to its character, an access that can avoid the village and connect to Carterton Road would alleviate traffic here.

The Neighbourhood Plan sets out that whilst no development should occur outside the settlement boundary, any development should retain and enhance the key components of the rural landscape character in the parish, making particular reference to the Landscape Character Assessment. Such development should adhere to the proposals set out within the Design Code but development in the countryside should adhere more strictly to the code set out below.

These include retention and enhancement of:

- The predominantly agricultural character and land use of the parish
- the openness of views and resulting inter-visibility within the parish
- the characteristic pattern of hedgerows and limited tree cover arranged along linear or rectilinear arrangements.

Policies in the NP also identify important views to be protected.

It has been stated above that views into open countryside are highly valued by local people and as being crucial to the identity of the village. The areas immediately east of the linear village allow for these views and the creation of a more dispersed pattern of development here would not be desirable. New roads, infrastructure and building would be resisted as a result.

It is noted that fields tend to be large and regular in this area of the wolds. Dating from Enclosure, they are generally bounded by dry stone walling or hedgerows.

Although just outside the Cotswolds AONB, Brize Norton is located within the

Limestone Wolds landscape area (see West Oxfordshire Design Guide) within the central area of the District, lying between the 85m and 200m contours, and is formed by the dip slope of the Cotswolds. It shares many of the characteristics of the AONB with a highly distinctive open rolling landform with fragments of the ancient Wychwood Forest, forming landscape elements of exceptional importance.

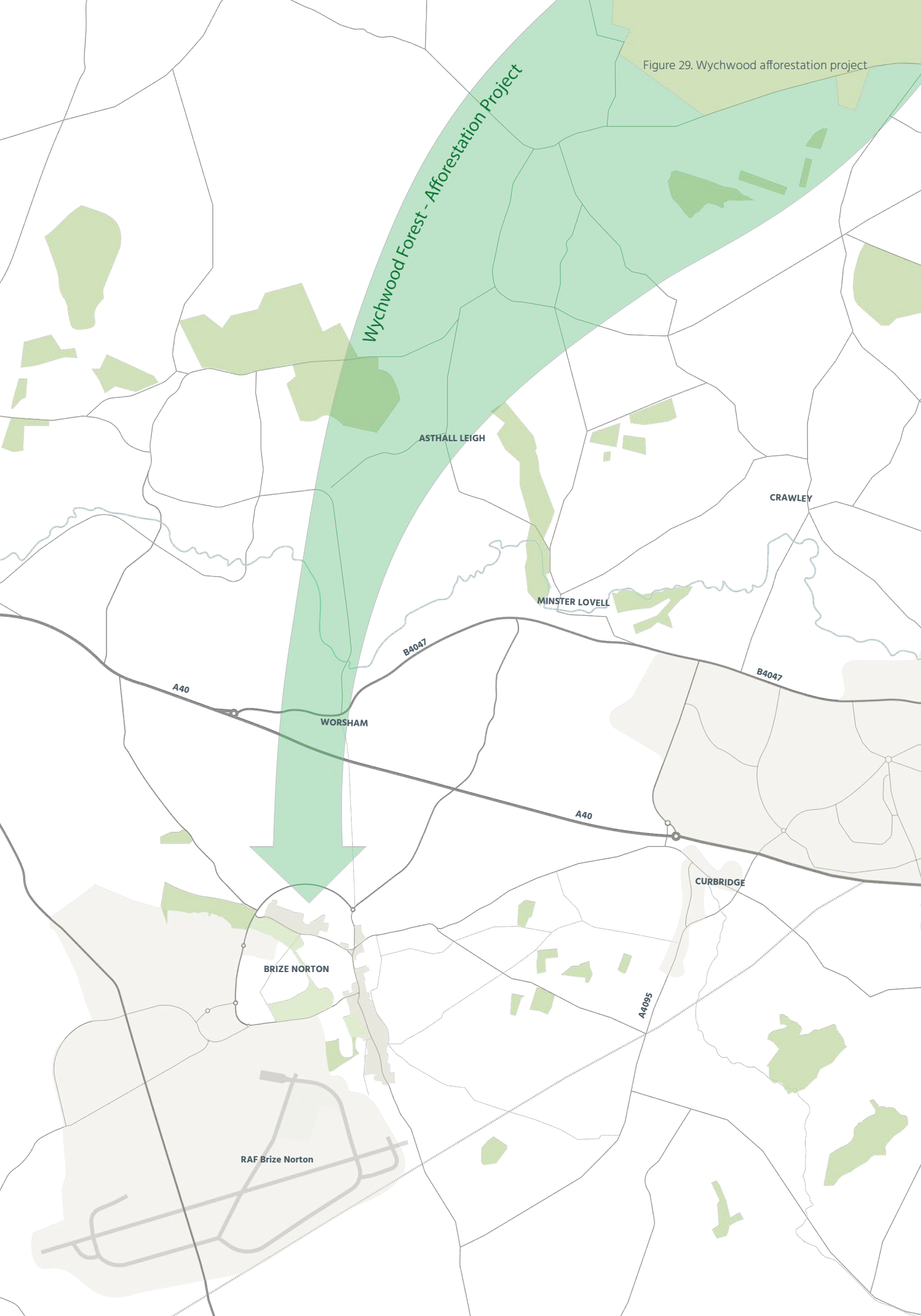


Figure 29. Wychwood afforestation project

To the north of the settlement the A40 runs along the ridgeline with land dropping away towards Brize Norton. This offers views which are now dominated by the RAF base and Carterton, the tallest structure in the village probably being barely visible (the church).

Significant nature recovery opportunities exist in the wider parish especially in the wolds area between the village and the A40 therefore and in this area planting or reafforestation should be encouraged. Native species should be utilized here including:

Key tree and hedgerow species:

- Beech *Fagus sylvatica*
- Ash *Fraxinus excelsior*
- Field Maple *Acer campestre*
- Hawthorn *Crataegus monogyna*

Significant species:

- Oak *Quercus robur*
- Sycamore *Acer pseudoplatanus*
- Natural features should also be enhanced to encourage biodiversity elsewhere as at Norton Ditch which forms the eastern boundary of the parish and at important PROW networks such as Ting Tang Lane.

In addition, higher density housing will be preferred to minimise land take with remaining site areas being retained as open land for recreation, biodiversity and agricultural useage albeit low intensity with benefits for biodiversity. Land may also be available for renewable energy provided every new home within the scheme is connected to and benefits from this resource.

A new community hub the location, design, construction and funding of which is to be agreed with the Parish Council via S106 agreement.

The developments must be walkable with full provision for cycling and walking including access to Brize Norton by a safe convenient non vehicular route.

Any new development should not be visible from the village of Brize Norton and this may be achieved through substantial woodland planting and retention

of existing native species and habitats of biodiversity value, utilisation of topographic features including avoidance of any construction on higher ground within a site boundary, avoidance of any light pollution visible from Brize Norton village sub areas. Pre-existing buildings should be included in any new development and refurbishment will be encouraged. Where older buildings are included in new development sites every effort should be made to integrate such buildings into the scheme either as homes or outbuildings/ garage space and where this is not possible materials should be re-used in the new development.

Hedgerows should be maintained and enhanced where these form a boundary to a development area or a corridor through the village out to the open countryside, including areas outside the settlement boundary.

Figure 30. Rocky Banks, Burford Road (West)



CODES FOR NEW DEVELOPMENTS IN RESIDUAL AREAS OF PARISH

4.5.1. Access and layout

- The existing network of roads north of the village offers a choice of routes and whilst there is a presumption against development outside the settlement boundary, new development or conversions/extensions might be proposed during the lifetime of the plan which might be accessed from these routes including Monahan Way (B4477).
- Layouts of any new developments should provide winding roads with staggered or fragmented building lines and individual styled buildings including corner plots and siting gables end on to side roads.



Figure 31. Gable end onto main road

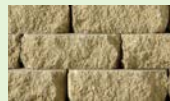
- Views of open countryside must be obtainable from all parts of new development. Mature trees, hedgerows, drystone field boundaries should be incorporated into any such schemes.

4.5.2. Buildings

- Houses should be provided with drives that have EV plug in connections.
- No flats, maisonettes or multi storey buildings will be permitted. Maximum heights will be 2-2.5 storeys.
- Any such proposals must ensure local materials, including slate roofs, Cotswold stone or reconstituted stone should be available and used or re-



Cotswold Stone



Reconstituted stone



Oxford red-brick only for chimney

used in such proposals including boundary dry stone walls and verges to

Figure 32. Station Road, Brize Norton



roads.

- Thatched roofs will not be acceptable neither will flat roof extensions. Existing pre 20th century structures including disused agricultural buildings should not be demolished but refurbished and integrated into any new schemes.

- Land used for development should be the minimum to achieve the proposed numbers of units and should reflect aspects of Brize Norton in terms of layout, fragmented building lines, orientation, plot occupancy and

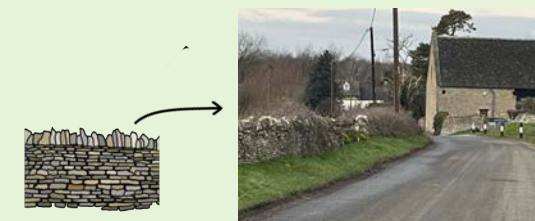


Figure 33. Dry stone walls are a traditional boundary marker densities, boundaries (dry stone walls) along road frontages.

4.5.3. Natural landscapes

- Garages and other parking should be discreetly hidden from roads and access drives should be surfaced with permeable materials to allow run off.
- Opportunities for biodiversity enhancement should be enhanced in



Figure 34. Example of discreet parking with permeable material

association with any new development but in particular scheme of 10 units or above should be required to set out proposals for stewardship schemes for long term maintenance.

CONCLUSION

This Design Code will be of benefit to all applicants proposing new build, extensions, changes of use and other forms of development. It will complement the Brize Norton Neighbourhood Plan and be in conformity with the West Oxfordshire District Plan and the West Oxfordshire Design Guide.

It should be read in conjunction with the policies in the Neighbourhood Plan.

